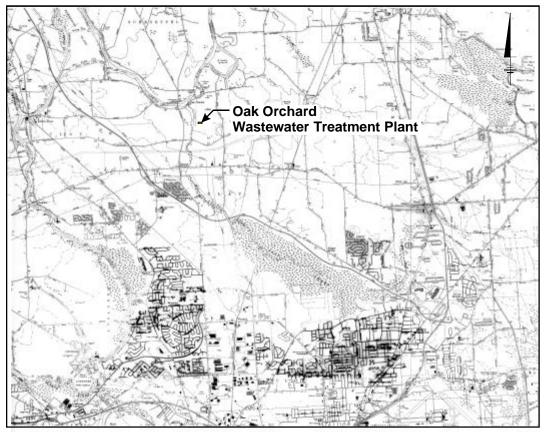
CONTRACT DRAWINGS

OAK ORCHARD WASTEWATER TREATMENT PLANT SITE IMPROVEMENTS

COUNTY OF ONONDAGA **DEPARTMENT OF DRAINAGE AND SANITATION** SYRACUSE, NEW YORK



REFERENCE: BASE MAP USGS 7.5 MIN QUAD SYRACUSE FAST AND WEST NEW YOR

LOCATION MAP

APPROVED BY ONONDAGA COUNTY DEPARTMENT OF DRAINAGE AND SANITATION Graphic Scale: 1 = 20

PROJECT NO. 587354

CONTRACT NO. 2

FEBRUARY 1999 REVISED MARCH 1999



INDEX TO DRAWINGS

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01/99 SYR-D54-DJH LBF

JOHN M. KARANIK, COMMISSIONER

LAN, LEGEND, AND NOTES AD SECTION AD - PLAN AND PROFILE, STATION 10+00 TO 19+00 AD - PLAN AND PROFILE. STATION 19+00 TO 28+50 AD - PLAN AND PROFILE. STATION 28+50 TO 37+00 AD - PLAN AND PROFILE, STATION 37+00 TO 44+99.25 AD MISCELLANEOUS DETAILS AND TABLE CE AND PROTECTION OF TRAFFIC NOTES ON AND WATER POLLUTION CONTROL DETAILS GUIDE RAIL PLAN AND ELEVATIONS PASS - PLAN AND PROFILE, STATION 0+00 TO 8+50 PASS - PLAN AND PROFILE, STATION 8+50 TO 17+19 PASS STRUCTURES OUS PROFILES AND DETAILS GOON BYPASS STRUCTURE DETAILS SITE PLAN STATION 28+50 TO 45+11 ATOR PLAN AND MISCELLANEOUS DETAILS PLAN, AND DETAILS

GENERAL NOTES:

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS AS MAY BE REQUIRED FOR THE CONSTRUCTION OF THIS PROJECT. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER AND APPROPRIATE DEPARTMENTS ALL REQUIRED DATA FOR APPROVAL. PRIOR TO THE 1. START OF CONSTRUCTION
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING THIS WORK WITH THE EXISTING UTILITY COMPANIES. THE CONTRACTOR SHALL CONTACT THE UNDERGROUND FACILITIES PROTECTIVE ORGANIZATION (UFPO) AT 1-800-962-7962 OR (315), 437-7333 AND NOTIFY THE PROPER UTILITY 2. COMPANIES TWO FULL WORKING DAYS PRIOR TO COMMENCEMENT OF CONSTRUCTION
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE SAFETY OF THE WORKERS AND OTHERS ON THIS PROJECT. 3.
- UNDERGROUND FACILITIES, STRUCTURES, AND UTILITIES HAVE BEEN PLOTTED FROM AVAILABLE SURVEYS AND RECORDS, AND THEREFORE THEIR LOCATIONS MUST BE CONSIDERED APPROXIMATE ONLY. THERE MAY BE OTHERS, THE EXISTENCE OF WHICH IS PRESENTLY NOT KNOWN 4.
- THE ACCURACY AND COMPLETENESS OF SUBSURFACE INFORMATION 5. SHOWN ON THESE PLANS IS NOT GUARANTEED. THE CONTRACTOR SHALL DETERMINE FOR HIMSELF, PRIOR TO BIDDING, THE ACCURACY AND COMPLETENESS OF SUCH INFORMATION.
- 7. RESTORED TO THEIR ORIGINAL CONDITION OR BETTER AS OUTLINED IN
- 8.

NO ALTERATIONS PERMITTED HEREON EXCEPT

AS PROVIDED LINDER SECTION 7209 SUBDIVISI

2 OF THE NEW YORK STATE EDUCATION LAW

- INVERT ELEVATIONS SHOWN AT MANHOLES ARE THE PIPE INVERTS 9. EXTENDED TO THE CENTERLINE OF EACH MANHOLE AND WERE COMPUTED USING THE DISTANCES AND GRADES BETWEEN THE CENTERLINE OF MANHOLES AS SHOWN.
- STATIONING AND GROUND ELEVATIONS SHOWN ON PROFILES IS ALONG 10. THE CENTER LINE OF THE ROADWAY ALIGNMENT.
- 11. ALL EXISTING DRAINAGE SYSTEMS, INCLUDING DITCHES AND CULVERTS, WITHIN THE CONTRACT LIMITS ARE TO BE CLEANED AND KEPT CLEAN AND FREE FLOWING FOR THE DURATION OF THE CONTRACT.
- 12. THE CONTRACTOR MUST ACQUAINT HIMSELF WITH DRAINAGE CHARACTERISTICS OF THE AREA SO THAT HE WILL PROGRESS HIS WORK EFFICIENTLY WITH FULL KNOWLEDGE OF THE POTENTIAL DRAINAGE PROBLEMS.
- 13. ALL ELEVATIONS REFER TO DATUM NGVD 1929.
- 14. MARKING OF UTILITY LOCATIONS AND NECESSARY MISCELLANEOUS EXCAVATION SHALL BE DONE PRIOR TO THE START OF CONSTRUCTION.
- 15. ALL UTILITY GATE VALVE BOXES DISTURBED BY CONSTRUCTION SHALL BE RESTORED AND ADJUSTED TO GRADE BY THE CONTRACTOR.

SPECIAL NOTES:

ANY DISCREPANCY BETWEEN THE CONTRACT PLANS AND THE 1. SPECIFICATIONS SHALL BE PROMPTLY BROUGHT TO THE ATTENTION OF THE ENGINEER PRIOR TO START OF CONSTRUCTION. THE ENGINEER WILL MAKE THE FINAL DECISION.

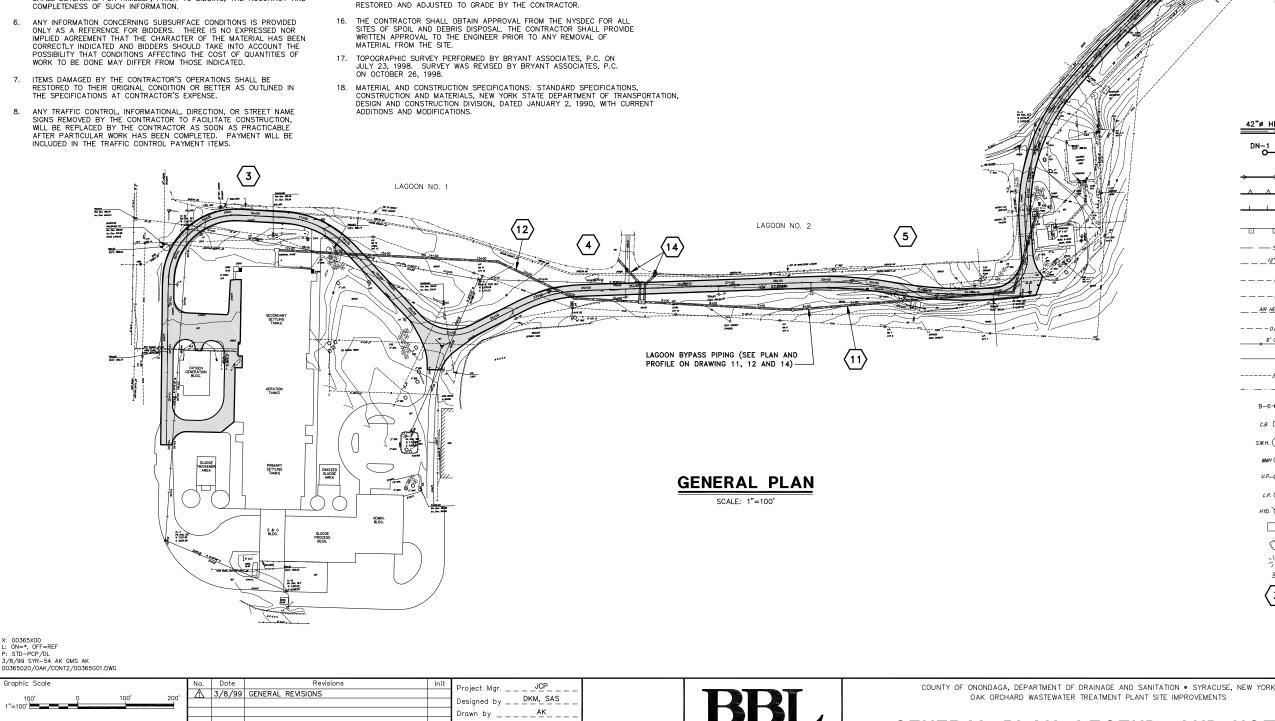
2. STANDARD WORK HOURS

BLASLAND, BOUCK & LEE, INC.

engineers & scientists

- A. ALL CONSTRUCTION ACTIVITIES ON THIS PROJECT ARE TO BE CARRIED OUT DURING STANDARD WORKING HOURS OF 8:00 A.M. TO 6:00 P.M.
- ANY REQUEST OF WAIVER FROM THE ABOVE REGULATION MUST BE в. SUBMITTED IN WRITING TO THE ENGINEER AND APPROPRIATE DEPARTMENTS FOR APPROVAL.
- THE CONTRACTOR SHALL PLAN AND COORDINATE HIS WORK SUCH THAT THERE IS MINIMUM INCONVENIENCE TO MOTORISTS. C.

(6)



Checked by ____JCP___

Prof. Eng. _ DONALD GEISSER

PE License _ 57879_N.Y.

ACCESS ROAD (SEE PLAN AND PROFILE ON DRAWING 3, 4, 5 AND 6)

> SECURITY ENTRANCE GATE (SEE DRAWING 17 AND 18)

LEGEND

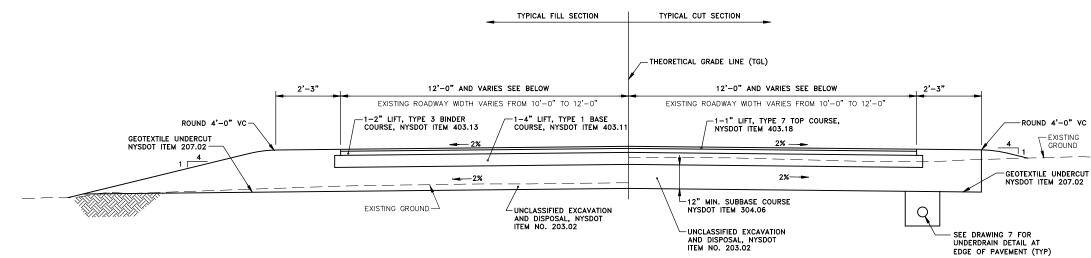
42"ø HDPE	
	LAGOON BYPASS PIPE
DN-1 DN-2 O	STORM SEWER MANHOLE, PIPE AND END SECTION
$\rightarrow \rightarrow \rightarrow \rightarrow$	DRAINAGE SWALE
	ACCESS ROAD CUT BOUNDARY
	ACCESS ROAD FILL BOUNDARY
••••	EXISTING BOX BEAM GUIDE RAIL
— — ss — —	EXISTING SANITARY SEWER LINE
12" <u>ST.</u>	EXISTING STORM SEWER LINE
c	EXISTING GAS LINE
w	EXISTING WATER LINE
<u>AIR_HEADER</u>	EXISTING AIR HEADER PIPE
	EXISTING OVER HEAD ELECTRIC LINE
<u> </u>	EXISTING FENCE LINE
	EXISTING EDGE OF ROADWAY PAVEMENT
390	EXISTING CONTOUR
	EXISTING EDGE OF LAGOON
B-6-	EXISTING SOIL BORING LOCATION
С.В.	EXISTING CATCH BASIN
s.м.н. ()	EXISTING SANITARY SEWER MANHOLE
имн 🛞	EXISTING WATER MANHOLE
U.P	EXISTING UTILITY POLE
L.P. 0	EXISTING LIGHT POLE
HYD. 🗹	EXISTING HYDRANT
	EXISTING BUILDING STRUCTURE
\bigcirc	EXISTING DECIDUOUS TREE
米	EXISTING EVERGREEN TREE
sign	EXISTING ROAD SIGN
3	DRAWING INDEX NUMBER

CONTRACT NO. 2

GENERAL PLAN, LEGEND, AND NOTES

File Number 003.65.101F

Date FEBRUARY 1999 Blasland, Bouck & Lee, Ir Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120



ACCESS ROAD RECONSTRUCTION WITHOUT SHOULDERS SECTION

NOT TO SCALE

RIGHT LANE TREATMENT

LEFT LANE TREATMENT

12' LT LANE	STA.	10+00.00 -	19+80.83	12' RT LANE	STA.	10+00.00 - 13+10.7	'0
LT LANE VARIES 12' TO 16'	STA.	19+80.83 -	20+20.83	RT LANE VARIES 12' TO 14'	STA.	13+10.70 - 13+40.7	70
16' LT LANE	STA.	20+20.83 -	21+49.50	14' RT LANE	STA.	13+40.70 - 15+67.4	45
LT LANE VARIES 16' TO 12'	STA.	21+49.50 -	21+89.50	RT LANE VARIES 14' TO 12'	STA.	15+67.45 - 15+97.4	¥5
12' LT LANE	STA.	21+89.50 -	32+16.45	12' RT LANE	STA.	15+97.45 - 16+32.4	19
LT LANE VARIES 12' TO 14'	STA.	32+16.45 -	32+46.45	RT LANE VARIES 12' TO 14'	STA.	16+32.49 - 16+62.4	19
14' LT LANE	STA.	32+46.45 -	33+68.69	14' RT LANE	STA.	16+62.49 - 18+13.6	33
LT LANE VARIES 14' TO 12'	STA.	33+68.69 -	33+98.69	RT LANE VARIES 14' TO 12'	STA.	18+13.63 - 18+43.6	33
12' LT LANE	STA.	33+98.69 -	42+90.00	12' RT LANE	STA.	18+43.63 - 20+03.1	11
LT LANE VARIES 12' TO 13.5'	STA.	42+90.00 -	43+15.00	RT LANE VARIES	STA.	20+03.11 - 21+60.0	00
13.5' LT LANE	STA.	43+15.00 -	43+65.00	12' RT LANE	STA.	21+60.00 - 32+72.6	54
LT LANE VARIES 13.5' TO 12'	STA.	43+65.00 -	43+90.00	RT LANE VARIES	STA.	32+72.64 - 33+67.2	21
12' LT LANE	STA.	43+90.00 -	44+27.27	12' RT LANE	STA.	33+67.21 - 42+90.0	00
LT LANE VARIES 14' TO 12'	STA.	44+27.27 -	44+47.27	RT LANE VARIES 12' TO 13.5'	STA.	42+90.00 - 43+15.0	00
LT LANE VARIES	STA.	44+47.27 -	44+99.25	13.5' RT LANE	STA.	43+15.00 - 43+65.0	00
				RT LANE VARIES 13.5' TO 12'	STA.	43+65.00 - 43+90.0	00

12' RT LANE

RT LANE VARIES

X: 003650.X00 L: ON=*, OFF=REF P: STD=PCP/DL 1/29/99 SYR=54-NES RJM AK 00365020/OAK/CONT2/00365C03.DWG

Graphic Scale Date Revisions No Project Mgr. _ _ _ JCP_ . Designed by _ _ _ DKM _ NOT TO SCALE Drawn by ____AK____ Checked by ____JAH____ NO ALTERATIONS PERMITTED HEREON EXCEPT Prof. Eng. _ DONALD GEISSER BLASLAND, BOUCK & LEE, INC. AS PROVIDED UNDER SECTION 7209 SUBDIVISIO 2 OF THE NEW YORK STATE EDUCATION LAW engineers & scientists

COUNTY OF ONONDAGA, DEPARTMENT OF DRAINAGE AN OAK ORCHARD WASTEWATER TREATMENT F

STA. 43+90.00 - 44+46.58

STA. 44+66.58 - 44+99.25

RT LANE VARIES 14' TO 12' STA. 44+47.27 - 44+67.27

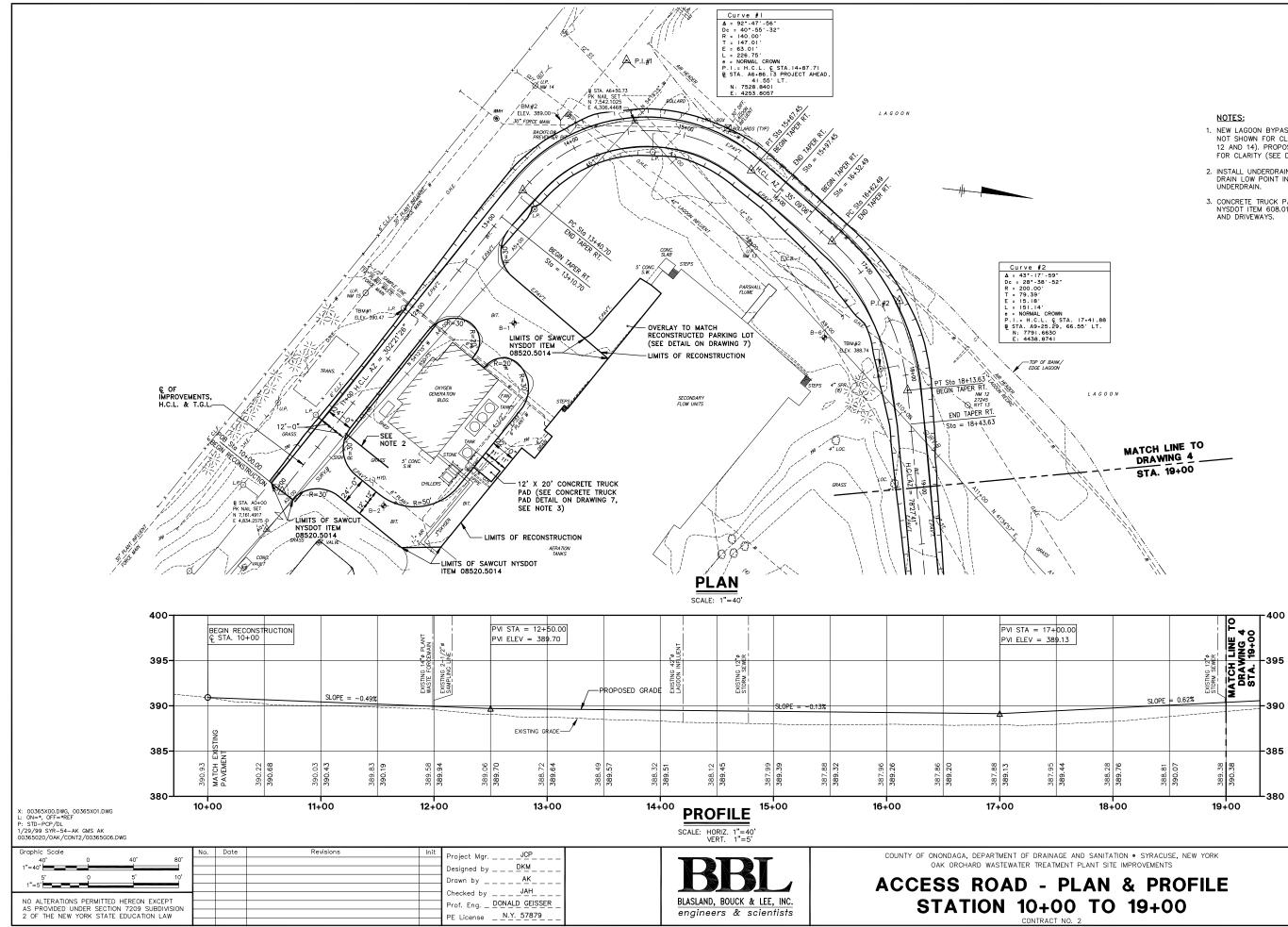
ACCESS ROAD SECTION

CONTRACT NO. 2

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PLA	٨NT	SITE	IMP	R	OVEMENTS			

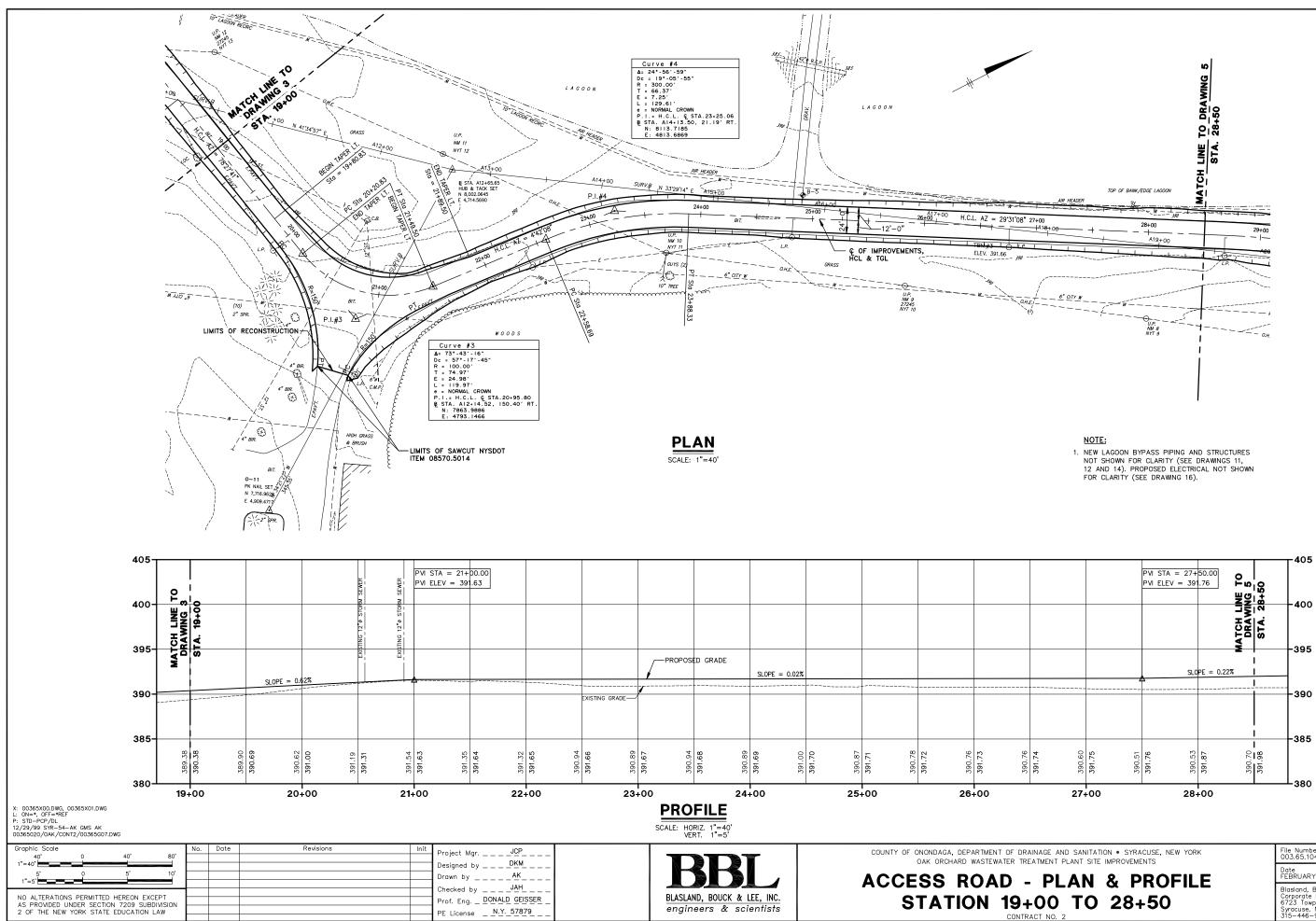
File Number 003.65.102F

Date FEBRUARY 1999 Blasland, Bouck & Lee, In Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315—446—9120



File Number 003.65.103 Date FEBRUARY 1999 Blasland, Bouck & Lee, In Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120

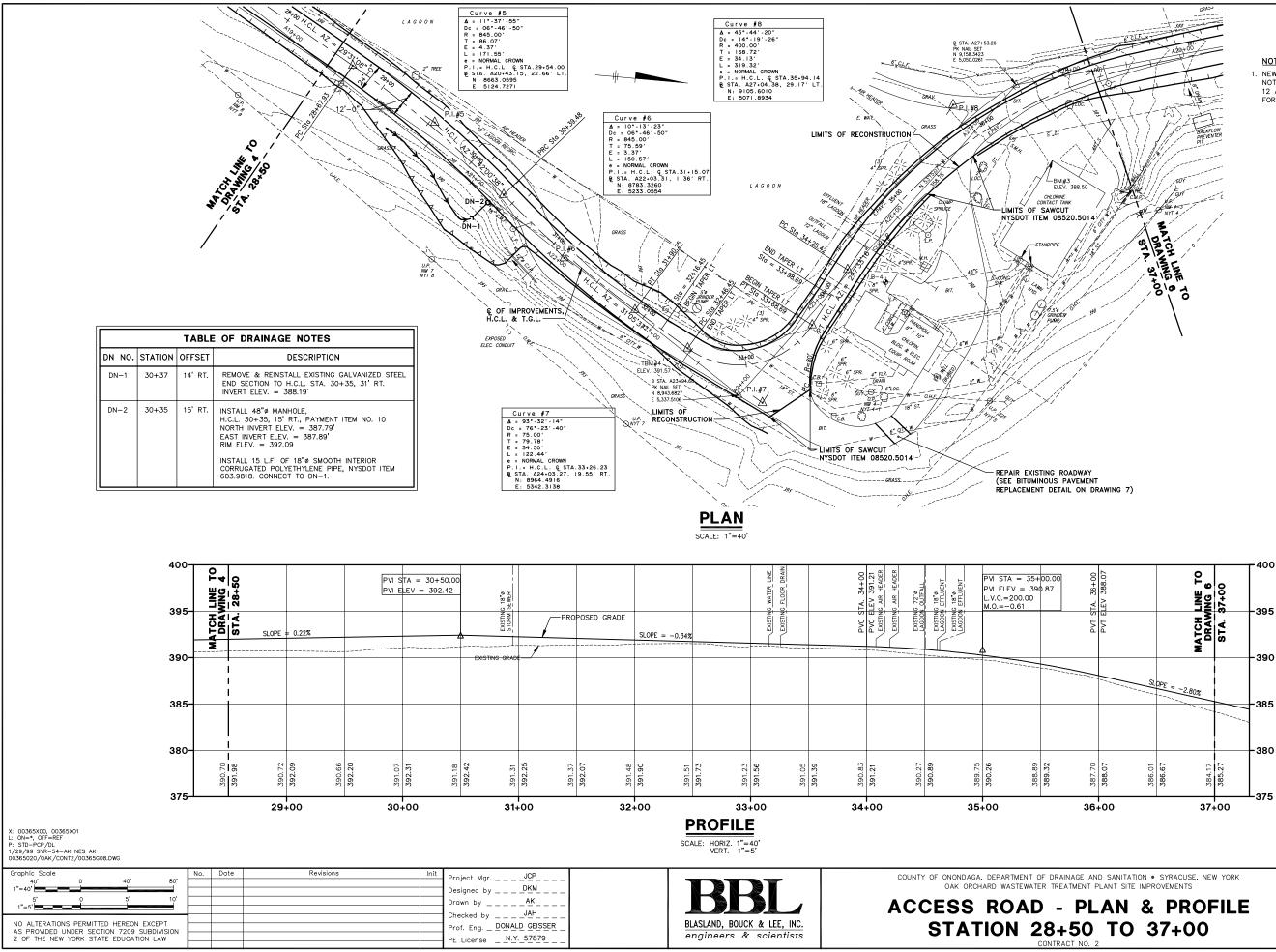
- 1. NEW LAGOON BYPASS PIPING AND STRUCTURES NOT SHOWN FOR CLARITY (SEE DRAWINGS 11, 12 AND 14). PROPOSED ELECTRICAL NOT SHOWN FOR CLARITY (SEE DRAWING 16).
- INSTALL UNDERDRAIN STA. 10+75 60' RIGHT TO DRAIN LOW POINT IN LAWN TO ACCESS ROAD UNDERDRAIN.
- CONCRETE TRUCK PAD TO BE PAID UNDER NYSDOT ITEM 608.0101 CONCRETE SIDEWALKS AND DRIVEWAYS.



Date FEBRUARY 1999 Blasland, Bouck & Lee, In Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120

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File Number 003.65.104F

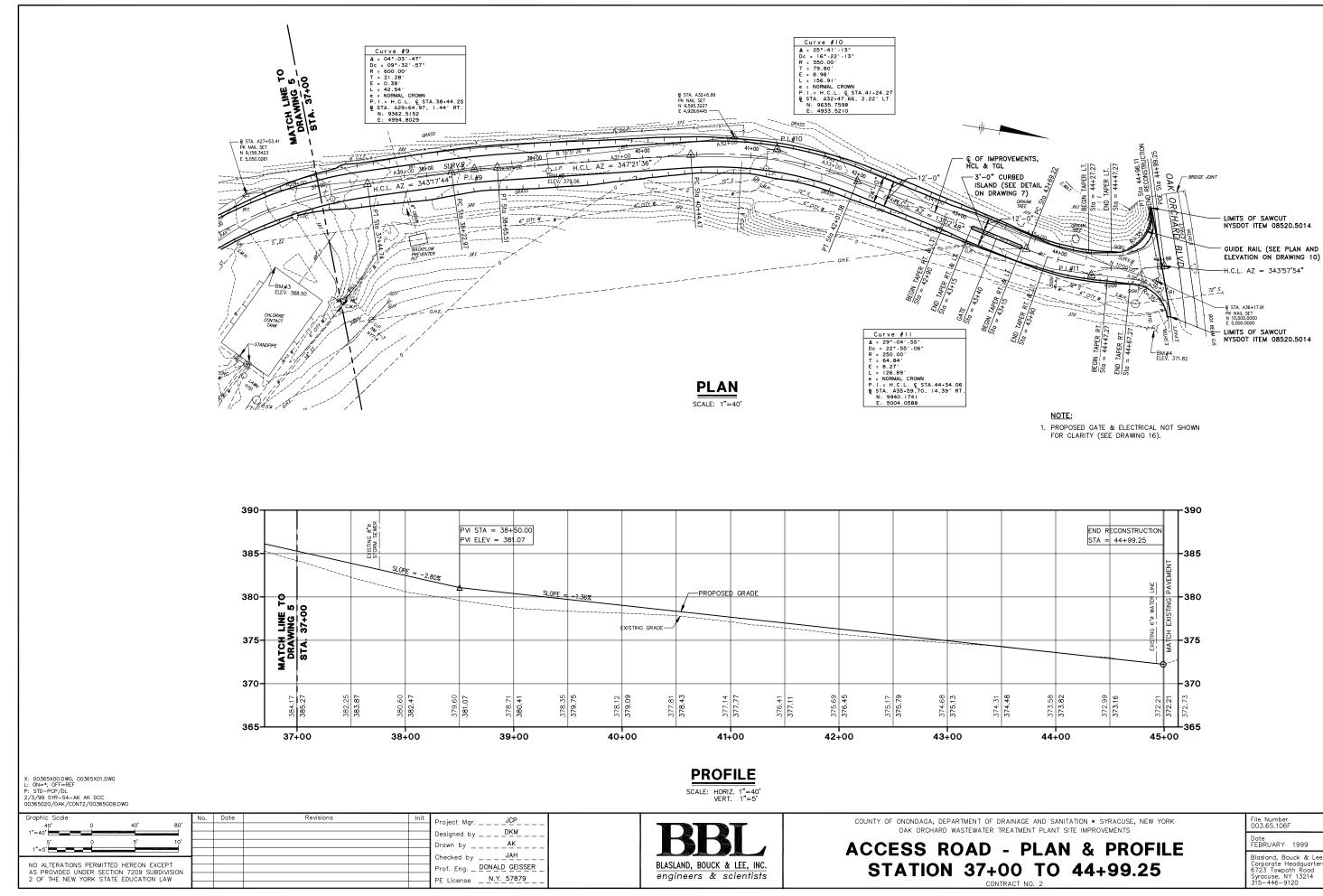


NOTE:

1. NEW LAGOON BYPASS PIPING AND STRUCTURES NOT SHOWN FOR CLARITY (SEE DRAWINGS 11, 12 AND 14). PROPOSED ELECTRICAL NOT SHOWN FOR CLARITY (SEE DRAWING 16).

FEBRUARY	1999
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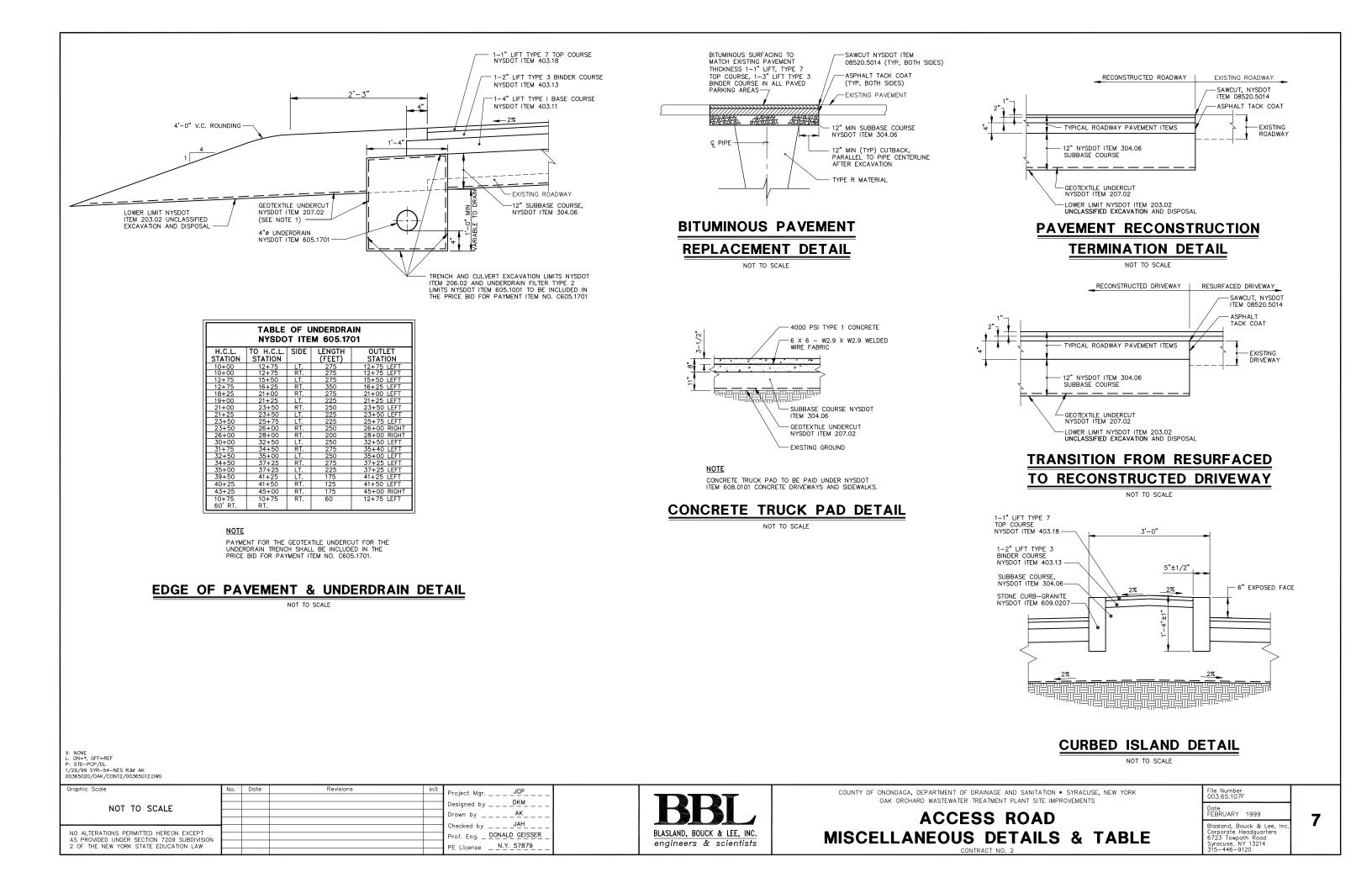
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FEBRUARY	1999





GENERAL

THE CONTRACTOR SHALL MAINTAIN TRAFFIC THROUGHOUT THE LENGTH AND DURATION OF THE CONTRACT IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 619 OF THE NYSDOT STANDARD SPECIFICATIONS AND THE NEW YORK STATE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (NYS MUTCD) OR AS ORDERED BY ENGINEER (AOBE).

FOR TYPICAL APPLICATIONS OF TRAFFIC CONTROL DEVICES IN CONSTRUCTION AREAS, THE PROVISIONS OF SUBCHAPTERS G AND H OF THE NYS MUTCD SHALL APPLY. WHERE OPTIONS EXIST FOR SIGN SHAPE, THE DIAMOND SHAPE SHALL BE USED. THE STANDARDS OF APPLICATION NOTED THEREIN AND ON THE PLANS ARE TO BE CONSIDERED MINIMUM STANDARDS.

LANE CLOSURES

THE CONTRACTOR SHALL SCHEDULE WORK SO THAT ALL TRAVEL LANES IN EACH DIRECTION ARE OPEN WHEN THE CONTRACTOR'S OPERATIONS ARE CLOSED DOWN OR SUBSTANTIALLY CLOSED DOWN. THE MINIMUM WIDTH OF A TRAVELED LANE SHALL BE 12 FEET.

THE CONTRACTOR SHALL MAINTAIN TWO-WAY TRAFFIC AND SHALL PROVIDE FLAGGERS. AOBE TO CONTROL TRAFFIC MOVEMENTS AT THE SITE OF WORK OPERATIONS REQUIRING TEMPORARY CLOSING OF PORTIONS OF THE TRAVELED WAY. ONE LANE OF TRAFFIC MUST BE MAINTAINED AT ALL TIMES.

SHORT DURATION LANE CLOSURES ARE DAILY LANE CLOSURES AND ARE COMPLETELY REMOVED AT THE END OF THE WORK DAY.

CONES, DRUMS, BARRICADES, VERTICAL PANELS, AND MARKERS

WHEN POSSIBLE, ALL CHANNELIZATION/DELINEATION DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM OF 2 FEET CLEARANCE TO THE TRAVELED WAY. THE CONTRACTOR SHALL MAKE CERTAIN THAT PLACEMENT OF CONES, DRUMS, VERTICAL PANELS, AND MARKERS OR BARRICADES SHALL NOT INTERFERE WITH SIGHT DISTANCE.

ANY CHANNELIZATION DEVICE POSTED DURING THE HOURS OF DARKNESS SHALL BE EITHER DRUMS OR 1 FOOT X 2 FOOT VERTICAL PANELS.

LONGITUDINAL SPACING SHALL BE 1 FOOT FOR EVERY MPH OF THE SPEED LIMIT BUT NOT MORE THAN 40 FEET, I.E., 30 MPH = 30 FOOT SPACING. REDUCED SPACING MAY BE REQUIRED AOBE

A DRUM MOUNTED WITH A TYPE B FLASHING HIGH INTENSITY WARNING LIGHT SHALL BE PLACED ON ALL APPROACHES TO A BUMP OR DIP IN THE PAVEMENT (A PAVEMENT CONDITION CONSIDERED BY THE ENGINEER TO BE SUFFICIENTLY ABRUPT ENOUGH TO CAUSE CONSIDERABLE DISCOMFORT, CARGO SHIFTING, OR DEFLECTION OF A VEHICLE FROM ITS TRUE COURSE AT PREVAILING DRIVING SPEEDS) THE COST OF THESE DRUMS AND LIGHTS SHALL BE INCLUDED IN THE PRICE BID FOR NYSDOT ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC.

WHERE POSSIBLE A LATERAL BUFFER SPACE OF 1 FOOT SHOULD BE PROVIDED BETWEEN THE WORK ZONE AND THE LINE OF DELINEATION DEVICES.

CONSTRUCTION INGRESS AND EGRESS

THE CONTRACTOR SHALL KEEP TO A MINIMUM MOVEMENTS OF CONSTRUCTION VEHICLES AND EQUIPMENT IN AND OUT OF DESIGNATED TRAVEL LANES. ONLY NECESSARY OR AUTHORIZED VEHICLES AS DETERMINED BY THE ENGINEER SHALL BE ALLOWED TO ENTER ANY PHASED WORK AREA.

VEHICLES AND/OR EQUIPMENT SHALL NOT BE PARKED WITHIN THE CLEAR ZONE, AS DETERMINED BY THE ENGINEER, ALONG A ROADWAY USED BY THE GENERAL PUBLIC (OR ANY OTHER AREAS DEEMED HAZARDOUS BY THE ENGINEER-IN-CHARGE).

VEHICLES PARKED BEHIND GUIDE RAIL/BARRIER SHALL BE PARKED BEYOND THE DEFLECTION DISTANCE OF THE RAIL.

WORK ZONE COORDINATION

WHEN TWO OR MORE WORK AREAS ARE ADJACENT, OVERLAPPING, OR ARE IN CLOSE PROXIMITY AS DETERMINED BY THE ENGINEER. THE CONTRACTOR SHALL ENSURE THERE IS NO CONFLICT IN SIGNING AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

THE CONTRACTOR SHALL COORDINATE ANY UTILITY WORK, SUBCONTRACTOR WORK, PUBLIC MAINTENANCE, OR OTHER CONSTRUCTION OPERATIONS IN THE AREA, AND IS RESPONSIBLE TO ENSURE PROPER MAINTENANCE AND PROTECTION OF TRAFFIC IS OBTAINED FOR ALL CONDITIONS.

MISCELLANEOUS

ALL ROADWAY AREAS TEMPORARILY CLOSED FOR TRENCH, CULVERT, OR CONDUIT EXCAVATION OR EXCAVATION REQUIRED FOR ALTERING DRAINAGE STRUCTURES SHALL BE REOPENED AT THE END OF THE WORK DAY. AT ALL TIMES WHEN WORK IS NOT IN BE ROGRESS, ALL OPENINGS SHALL BE COVERED WITH ANCHORED STEEL PLATES OR SHALL BE BACKFILLED IN ACCORDANCE WITH THE SPECIFICATIONS AS ORDERED BY THE ENGINEER. THE TEMPORARY RESTORATION SHALL BE COMPLETED WITH A LAYER OF PLANT MIX BITUMINOUS MATERIAL UP TO A LEVEL OF THE ADJACENT UNEXCAVATED AREA TO THE SATISFACTION OF THE ENGINEER. IF ANCHORED STEEL PLATES ARE THE CHOSEN OPTION, THE CONTRACTOR SHALL SUBMIT THE PROPOSED METHOD OF ANCHORING THE STEEL PLATES TO THE ENGINEER FOR APPROVAL PRIOR TO STARTING ANY ROADWAY EXCAVATION. THE STEEL PLATES SHALL BE RAMPED WITH A BITUMINOUS MATERIAL PRIOR TO OPENING THE LANE. THE COST OF THIS WORK, INCLUDING ALL LABOR, MATERIALS, EQUIPMENT, AND ANY REEXCAVATION OF TEMPORARY RESTORED AREAS SHALL BE INCLUDED IN THE PRICE BID FOR NYSDOT ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC

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GUIDE RAIL WORK

ALL FREE ENDS OF GUIDE RAIL SHALL BE TEMPORARILY TERMINATED IN THE SAME METHOD AS SUBSECTION 619-3.01 J in the Standard Specifications. The cost of this work shall be included in the price bid for NySDOT ITEM 619.01 basic maintenance and PROTECTION OF TRAFFIC

12 IN X 24 IN VERTICAL PANELS (C6-1, C6-2 IN THE NYS MUTCD) MOUNTED ON 2 IN X 2 IN WOODEN STAKES SHALL BE PROVIDED AT 40 FT INTERVALS WHEN RAIL IS DOWN AOBE. THE COST SHALL BE INCLUDED IN THE PRICE BID FOR FOR NYSDOT ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC

PRELIMINARY WORK

PRIOR TO THE START OF ANY WORK OPERATIONS, ALL RELATED WORK FOR PROPOSED MAINTENANCE AND PROTECTION OF TRAFFIC, AS DETERMINED BY THE ENGINEER, SHALL BE COMPLETE. THIS INCLUDES, BUT IS NOT LIMITED TO: ALL SIGNS, DELINEATION (CONES, DRUMS, ETC), FLAGGERS, PAVEMENT MODIFICATION, AND ANY OTHER RELATED WORK.

FLAGGER

WHEN FLAGGERS ARE BEING USED FOR TRAFFIC CONTROL PURPOSES. FLAGGER SIGNS (W8-22F) SHALL BE PLACED AOBE ON ALL APPROACHES TO THE AREAS BEING FLAGGED. THE CL FLAGGER SIGNS SHALL BE INCLUDED IN THE PRICE BID FOR NYSDOT ITEM 619.01 BASIC THE COST OF MAINTENANCE AND PROTECTION OF TRAFFIC. FLAGGER SIGNS SHALL NOT BE USED FOR BRIEF PERIODS OF INCIDENTAL FLAGGING. FLAGGER SIGNS SHALL NOT BE VISIBLE WHEN FLAGGERS ARE NOT BEING USED.

STOP / SLOW PADDLES SHALL CONFORM TO THE REQUIREMENTS IN THE NYS MUTCH BUT SHALL BE A MINIMUM WIDTH OF 2'-O. THE COST IF THESE PADDLES AND ALL FLAGGERS SHALL BE INCLUDED IN THE PRICE BID FOR FOR NYSDOT ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC

FLAGGERS SHALL NOT POSITION THEMSELVES DIRECTLY IN THE PATH OF ONCOMING TRAFFIC. AT ALL TIMES THE CONTRACTOR SHALL PROVIDE FLAGGERS WITH A CLEAR ESCAPE PATH, FREE OF PARKED VEHICLES, EQUIPMENT OR OTHER OBSTACLES. A SUFFICIENT NUMBER OF FLAGGERS SHALL BE UTILIZED, AS DIRECTED BY THE ENGINEER, SUCH THAT EACH FLAGGER SHALL CONTROL NO MORE THAN ONE APPROACH OF TRAFFIC. WHEN SIGNALING, FLAGGERS SHALL USE THEIR FREE HAND FOR EMPHASIS, AS DESCRIBED UNDER SECTION 293.3 OF THE MUTCD.

THE CONTRACTOR SHALL PROVIDE FLAGGERS WHERE SIGHT DISTANCES ARE IMPAIRED BY THE OPERATION OR ANY OTHER SITUATION AOBE.

FLAGGERS SHALL BE REQUIRED TO USE TWO-WAY, WALKIE-TALKIES, OR OTHER FORMS OF ENHANCED COMMUNICATION WHEN ONE FLAGGER IS NOT VISIBLE TO THE OTHER OR IF THE ENGINEER DEEMS IT NECESSARY. ALL COSTS SHALL BE INCLUDED IN THE BID PRICE FOR NYSDOT ITEM 619.01 BASIC MAINTENANCE AND PROTECTION OF TRAFFIC.

TRAFFIC MAINTENANCE NOTES

- ALL EXISTING ROADS INTERSECTING THE HIGHWAY SHALL REMAIN OPEN TO TWO-WAY 1. TRAFFIC AT ALL TIMES UNLESS AUTHORIZED BY THE ENGINEER
- 2. IN ORDER TO MAINTAIN EFFECTIVE TRAFFIC CONTROL, THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTINUOUS MAINTENANCE: MAKING SURE ALL SIGNS CONES, BARRICADES, ETC. ARE IN PLACE AND IN GOOD CONDITION. THE SOLE JUDGE OF THE EFFECTIVENESS OF THE CONTRACTOR'S EFFORTS TOWARDS PROTECTION OF TRAFFIC AND PERSONNEL SHALL BE THE ENGINEER
- 3. THE CONTRACTOR SHALL NOT PARK HIS EQUIPMENT OR STORE MATERIAL OVERNIGHT WHERE IT IS DEEMED BY THE ENGINEER TO BE A SAFETY HAZARD TO THE PUBLIC. GENERALLY, 30 FT OF CLEAR AREA BETWEEN THE EDGE OF PAVEMENT TO STORED MATERIAL OR EQUIPMENT IS TO BE MAINTAINED
- 4. THE CONTRACTOR SHALL TAKE EVERY PRECAUTION TO AVOID DAMAGING EXISTING PAVEMENT, CURB AND SIDEWALKS WHEN IT IS NECESSARY FOR HIM TO MOVE HIS EQUIPMENT. IN THIS REGARD, HE SHALL OBSERVE ALL THE RULES AND REGULATIONS AND DIRECTIONS OF THE LOCAL AUTHORITIES RELATIVE TO SUCH HANDLING OF EQUIPMENT AND TAKE SUCH PROTECTIVE MEASURES AS MAY BE ORDERED BY THE ENGINEER. PAVEMENT, CURB AND SIDEWALK OUTSIDE OF THE LIMIT OF WORK DAMAGED BY THE CONTRACTOR THROUGH DISREGARD OF THESE PROVISIONS, SHALL BE RESTORED AT HIS/HER SOLE COST AND EXPENSE TO EQUAL OR BETTER CONDITION

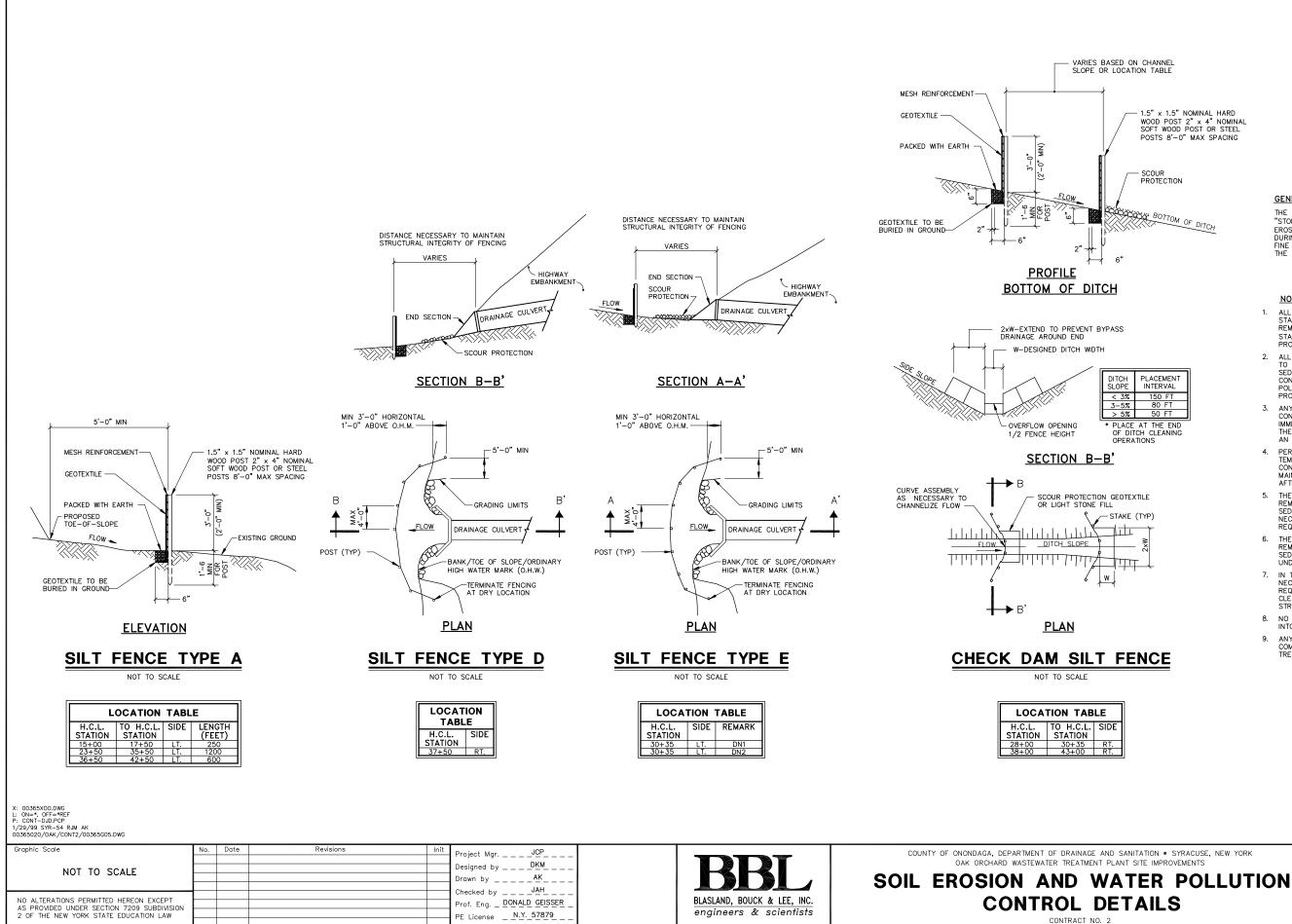
Graphic Scale	No.	Date	Revisions	Init	Project Mgr <u>JCP</u>		COUNTY OF ONONDAGA, DEPARTMENT OF DRAINAGE
					Designed by DKM		OAK ORCHARD WASTEWATER TREATMEN
					Drawn_byAK		MAINTENANCE &
	-			-			
NO ALTERATIONS PERMITTED HEREON EXCEPT AS PROVIDED UNDER SECTION 7209 SUBDIVISION					Prof. Eng DONALD GEISSER _	BLASLAND, BOUCK & LEE, INC.	OF TRAFFIC
2 OF THE NEW YORK STATE EDUCATION LAW	<u> </u>			_	PE License	engineers & scientists	CONTRACT NO



GE AND SANITATION . SYRACUSE, NEW YORK VENT PLANT SITE IMPROVEMENTS

> Date FEBRUARY 1999 Blasland, Bouck & Lee, I Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120

File Number 003.65.108F



DITCH SLOPE	PLACEMENT INTERVAL
< 3%	150 FT
3-5%	80 FT
> 5%	50 FT
	AT THE END TCH CLEANING TIONS

GENERAL NOTE

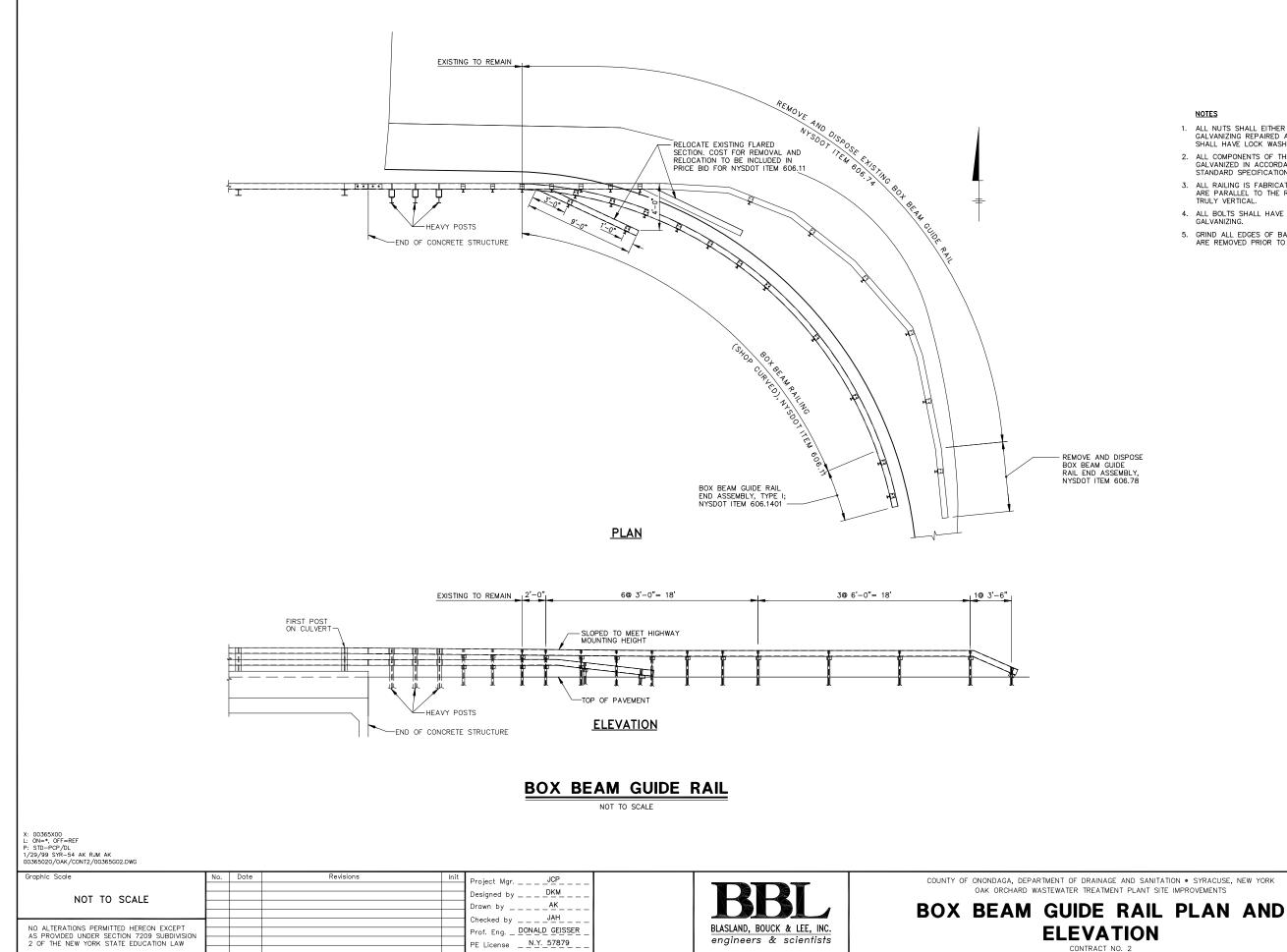
THE ELEMENTS OF THIS PAGE KNOWN AS "STORMWATER POLLUTION PREVENTION AND EROSION CONTROL PLAN" SHALL BE USED DURING CONSTRUCTION IN ANY AREA WHERE FINE MATERIALS MAY ENTER THE WATERS OF THE STATE OF NEW YORK.

NOTES

- ALL CONTROLS SHALL BE PLACED PRIOR TO STARTING EARTHWORK OPERATIONS AND SHALL REMAIN IN PLACE UNTIL THE NEW SLOPES ARE STABILIZED WITH SEEDING AND/OR SLOPE PROTECTION.
- ALL NECESSARY PRECAUTIONS SHALL BE TAKEN TO PREVENT MIGRATION INTO WATER BY SILT, SEDIMENT, FUELS, SOLVENTS, LUBRICANTS, CONCRETE LEACHATE OR ANY OTHER POLLUTANT ASSOCIATED WITH CONSTRUCTION PROCEDURES.
- 3. ANY DEBRIS OR EXCESS MATERIALS FROM CONSTRUCTION OF THIS PROJECT SHALL BE IMMEDIATELY AND COMPLETELY REMOVED FROM THE BED AND BANKS OF ALL WATER AREAS TO AN APPROPRIATE UPLAND AREA FOR DISPOSAL.
- 4. PERIODIC CLEANING AND INSPECTION OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL DEVICES WILL BE NECESSARY. MAINTENANCE OF THESE DEVICES IS REQUIRED AFTER ANY STORM EVENT AFTER ANY STORM EVENT.
- 5. THE COST OF INSTALLING, CLEANING AND REMOVING TEMPORARY SOIL EROSION AND SEDIMENT CONTROL DEVICES WILL BE NECESSARY. MAINTENANCE OF THESE DEVICES IS REQUIRED AFTER ANY STORM EVENT.
- THE COST OF INSTALLING, CLEANING AND REMOVING TEMPORARY SOIL EROSION AND 6. SEDIMENT CONTROL DEVICES SHALL BE PAID FOR UNDER THE ITEMS SHOWN.
- 7. IN THE EVENT DEWATERING OPERATION BECOMES RECESSARY, A SETLING BASIN WILL BE REQUIRED UNLESS THE PUMP DISCHARGE IS AS CLEAR AND FREE OF SEDIMENT AS THE FLOWING STREAM
- 8. NO DISCHARGE OF TEMPORARY FILL MATERIAL INTO ANY WATERWAY IS PERMITTED.
- ANY ACCESS WAYS OR DETOURS SHALL BE COMPLETELY ISOLATED WITH EROSION CONTROL 9. TREATMENTS.

File Number 003.65.109F Date FEBRUARY 1999

Blasland, Bouck & Lee, Ir Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120



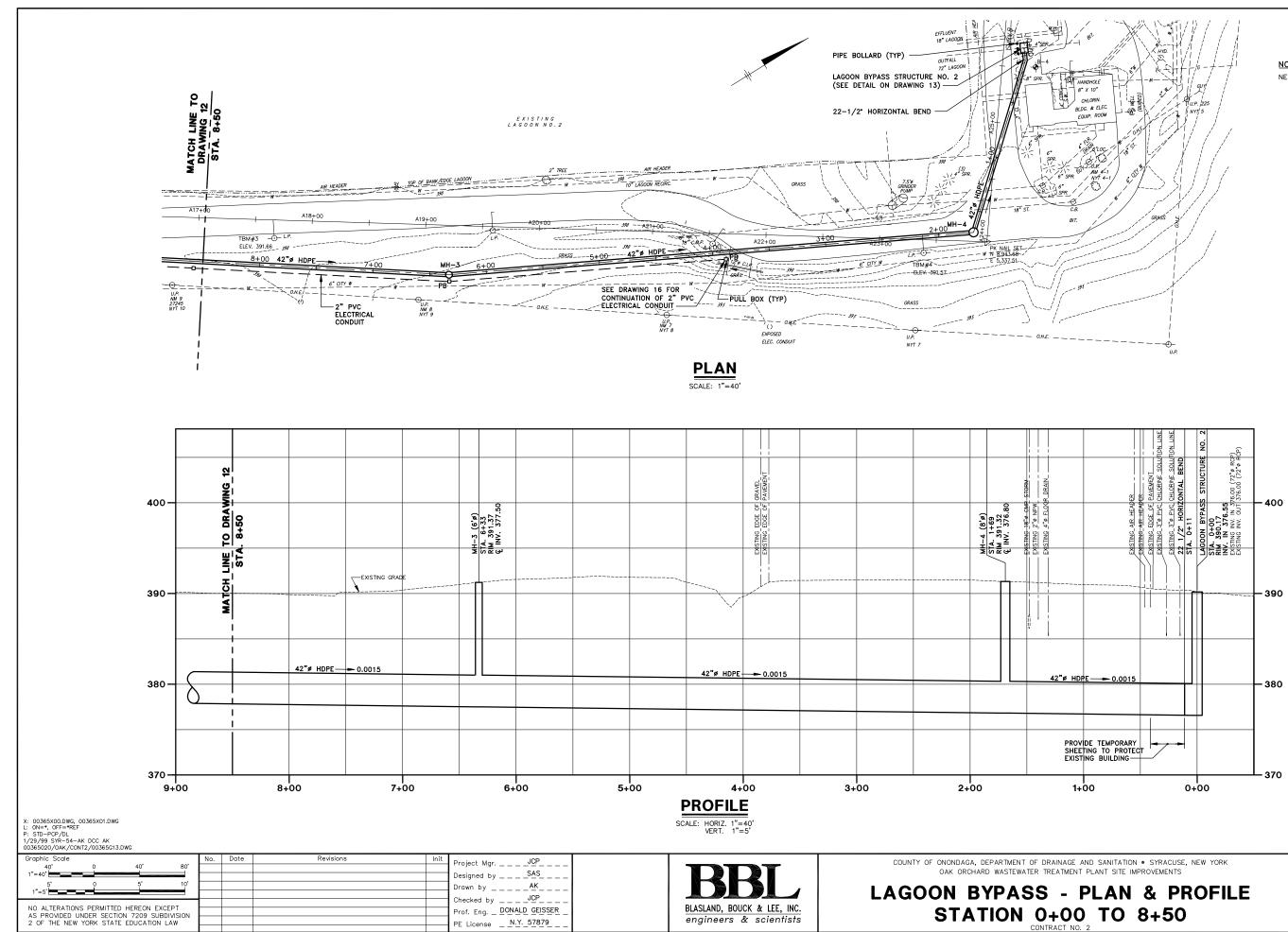
<u>NOTES</u>

- ALL NUTS SHALL EITHER BE TACK WELDED IN PLACE AND THE GALVANIZING REPAIRED ACCORDING TO SUBSECTION 719-01 OR SHALL HAVE LOCK WASHERS.
- ALL COMPONENTS OF THE RAILING SYSTEM SHALL BE GALVANIZED IN ACCORDANCE WITH SECTION 719-01 OF THE STANDARD SPECIFICATIONS AFTER FABRICATION.
- ALL RAILING IS FABRICATED AND ERECTED SO THAT THE RAILS ARE PARALLEL TO THE ROADWAY AND SO THAT THE POSTS ARE TRULY VERTICAL.
- 4. ALL BOLTS SHALL HAVE A CLASS 2A THREAD FIT PRIOR TO GALVANIZING.
- 5. GRIND ALL EDGES OF BASE PLATES SO THAT ALL SHARP EDGES ARE REMOVED PRIOR TO GALVANIZING.

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PL/	ANT	SITE	IMP	R	OVEMENTS			

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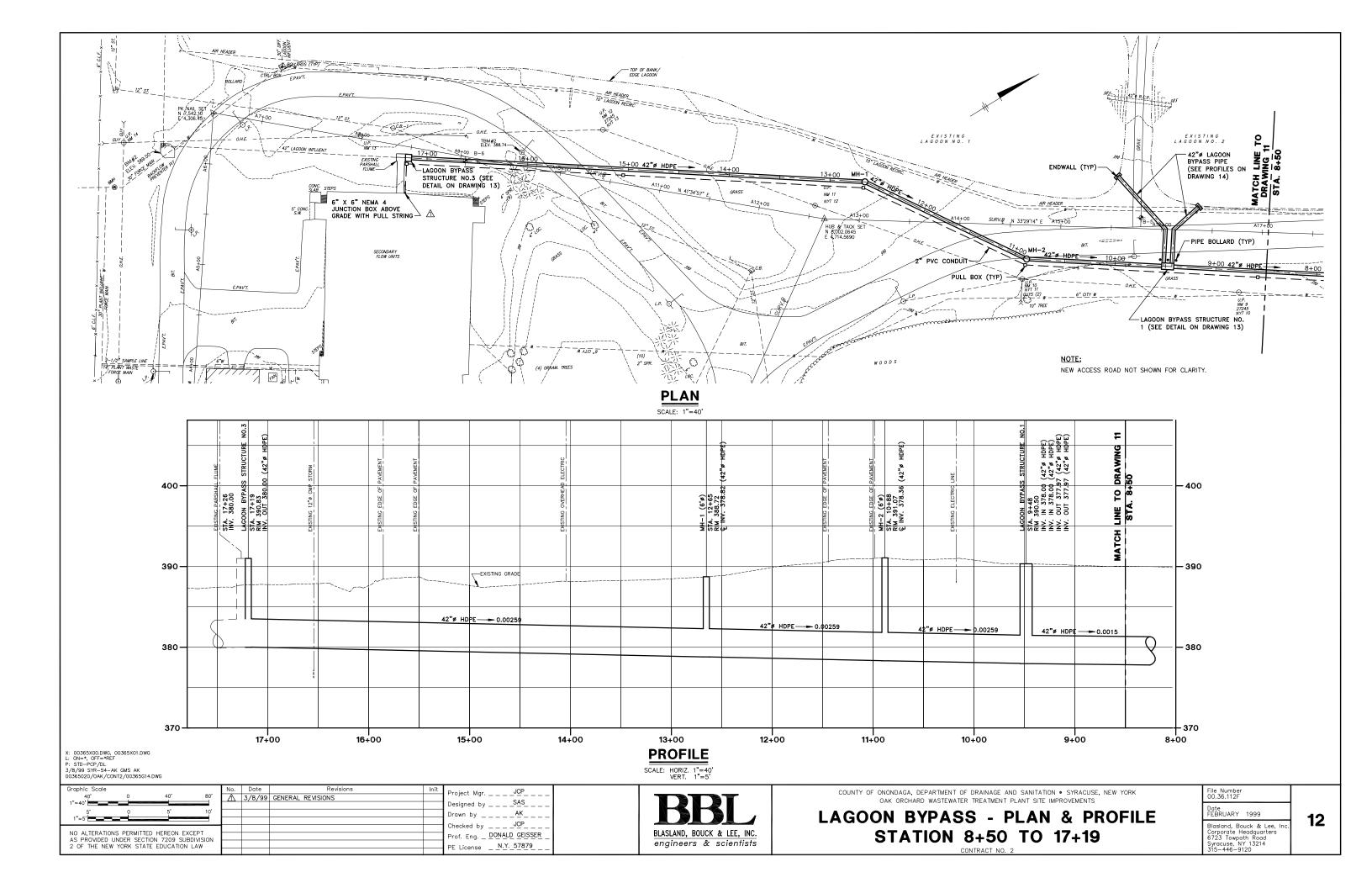


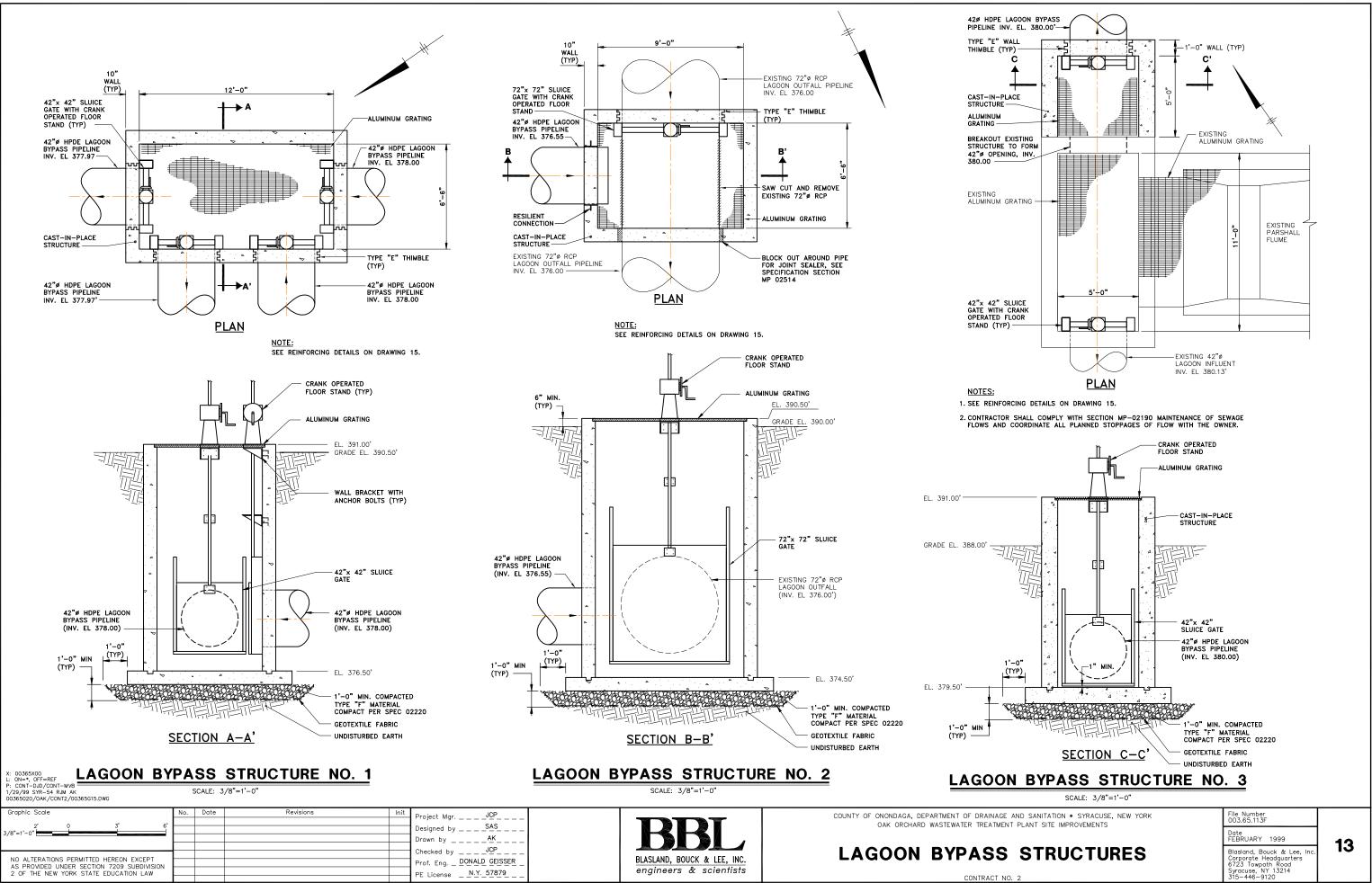
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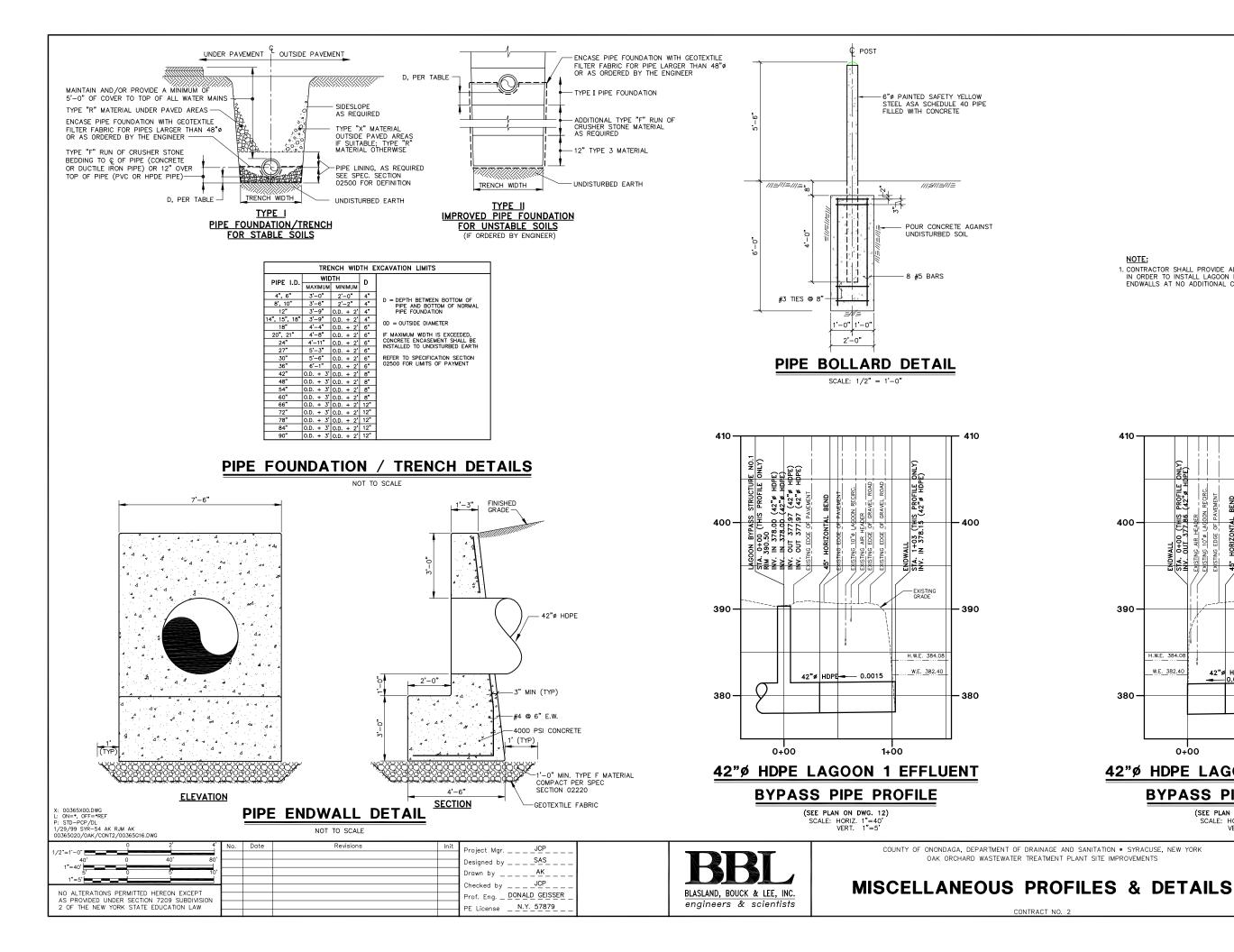
File Number 003.65.111F

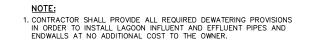
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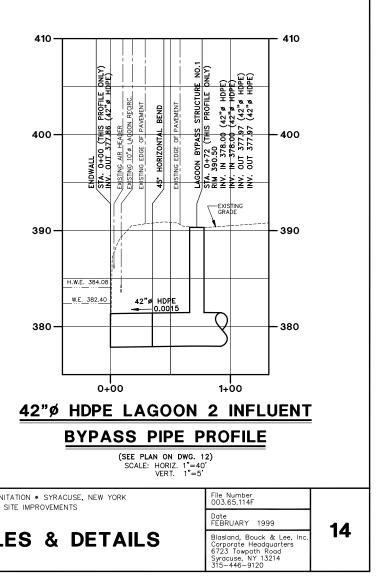
NOTE: NEW ACCESS ROAD NOT SHOWN FOR CLARITY.

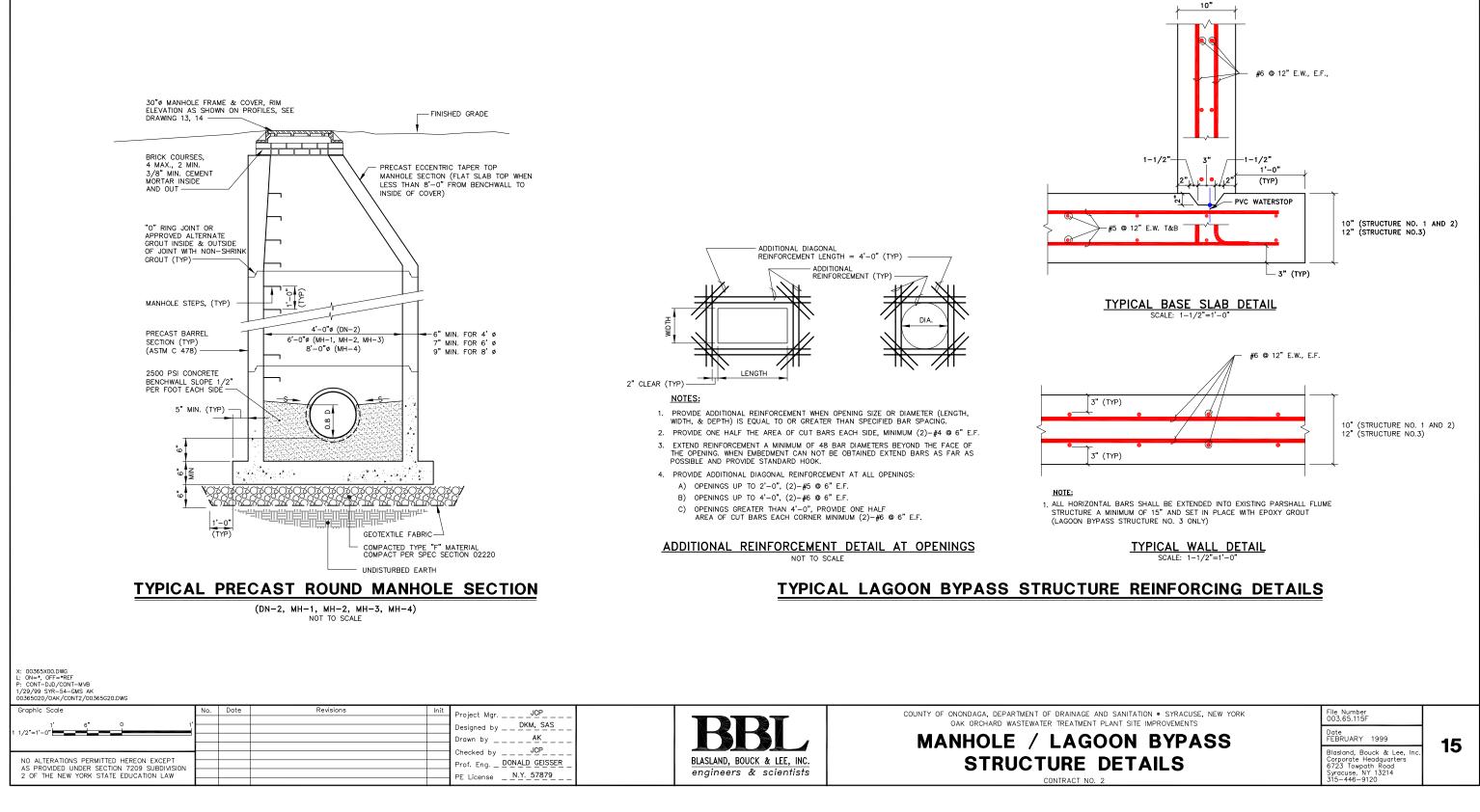


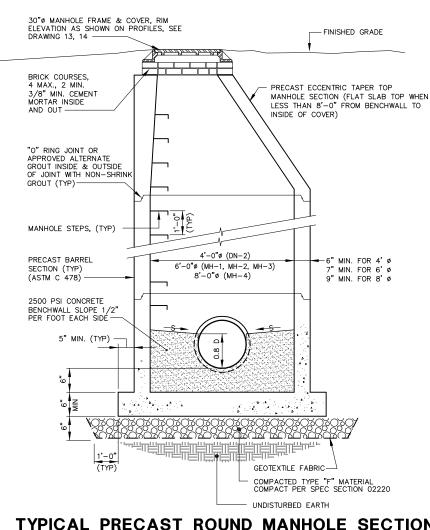


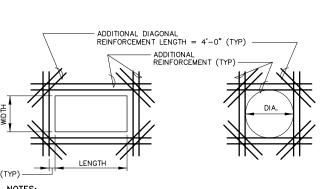


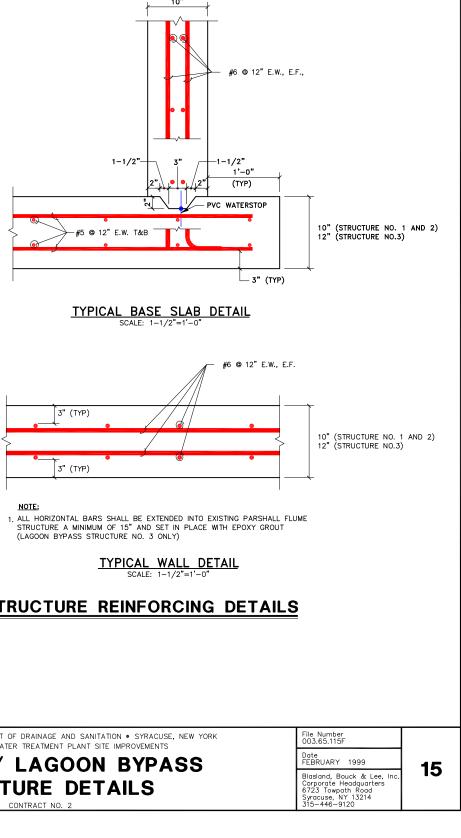


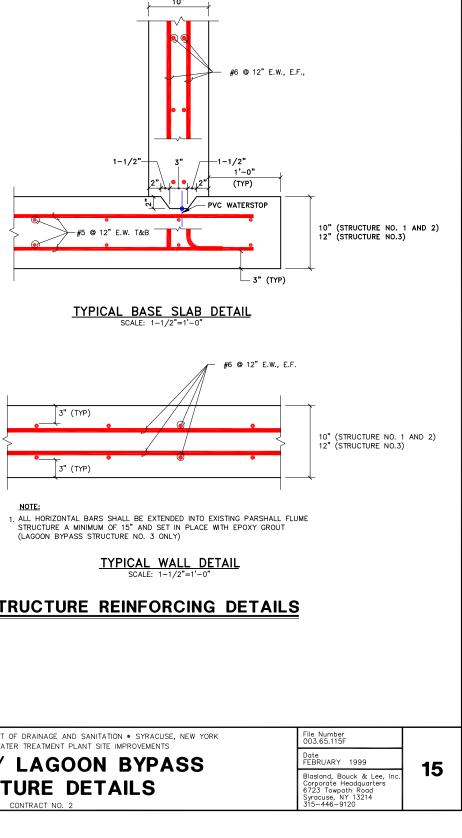


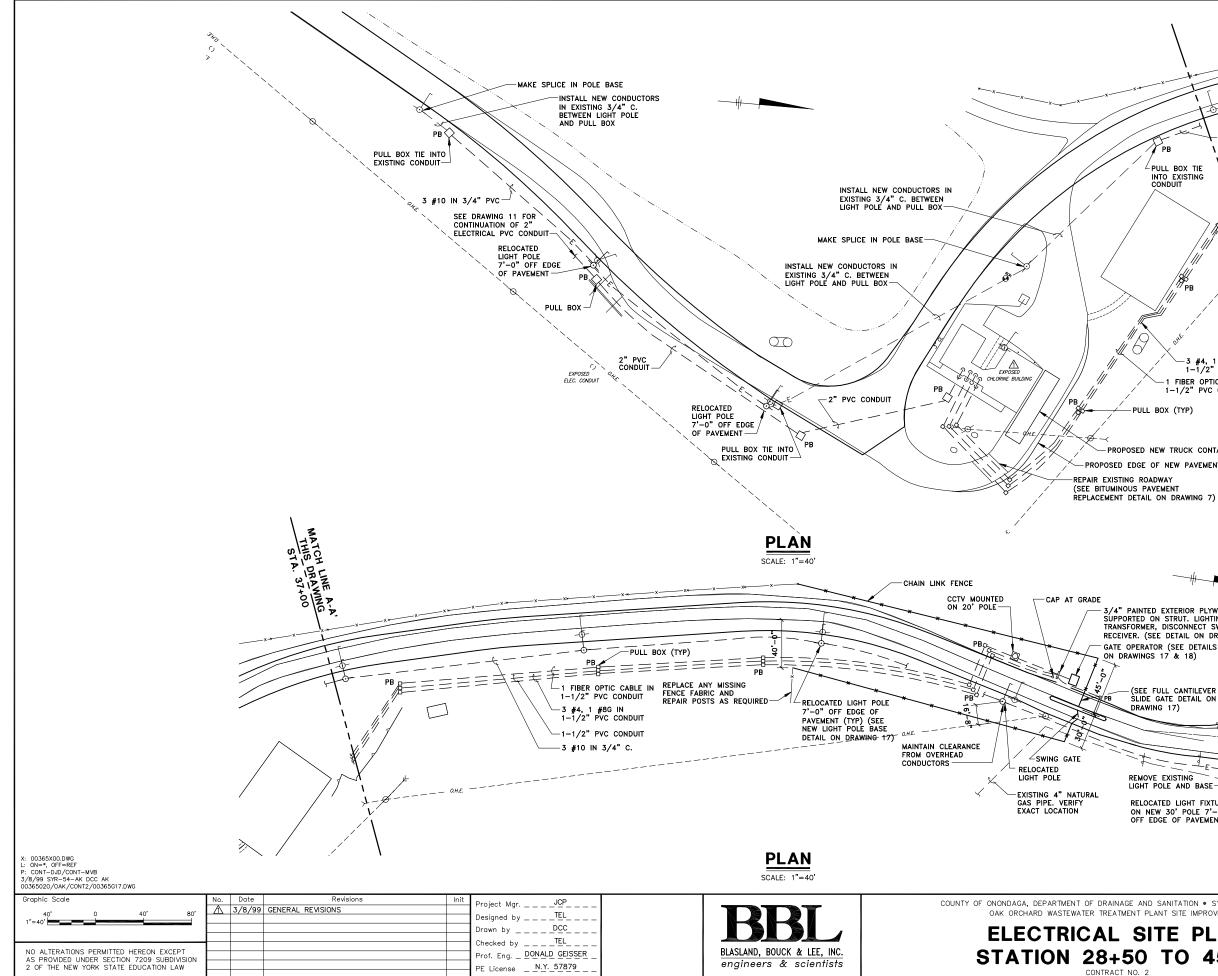












x
X
3 #10 IN PB \3/4" PVC
PULL BOX TIE
THIS DE PB
ATCH LINE A.A. STA. 37+00 out
3 #4, 1 #8G IN 1-1/2" PVC CONDUIT 1 FIBER OPTIC CABLE IN 1-1/2" PVC CONDUIT
LL BOX (TYP)
D NEW TRUCK CONTAINMENT PAD (BY OTHERS)

5) PROPOSED EDGE OF NEW PAVEMENT (BY OTHERS)

- 3/4" PAINTED EXTERIOR PLYWOOD SUPPORTED ON STRUT. LIGHTING PANEL, TRANSFORMER, DISCONNECT SWITCHES AND GATE RECEIVER. (SEE DETAIL ON DRAWING 18) GATE OPERATOR (SEE DETAILS ON DRAWINGS 17 & 18) - (SEE FULL CANTILEVER SLIDE GATE DETAIL ON DRAWING 17) REMOVE EXISTING RELOCATED LIGHT FIXTURE ON NEW 30' POLE 7'-0" OFF EDGE OF PAVEMENT-

ND SANITATION • SYRACUSE, NEW YORK PLANT SITE IMPROVEMENTS	File Number 003.65.116F	
SITE PLAN	Date FEBRUARY 1999	16
D TO 45+11	Blasland, Bouck & Lee, Inc. Corporate Headquarters 6723 Towpath Road Syracuse, NY 13214 315-446-9120	0

